



District 11 International Border Area

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District 11 covers the international border between California and Mexico, from coastal urban San Diego County east across the Imperial County to the state's border with Arizona. California's border region includes San Diego County (population 3,095,313) represented by the San Diego Association of Governments (SANDAG), and Imperial County (population 174,528) one of the six counties represented by the Southern California Association of Governments (SCAG), the nation's largest metropolitan planning organization, and the Imperial County Transportation Commission (ICTC), a state-designated Regional Transportation Planning Agency (RTPA).

INTRODUCTION

Caltrans District 11 International Border Studies Branch (IBSB) coordinates people and freight transportation planning efforts between California and Baja California. The IBSB's goal is to reduce congestion and to improve mobility in California's international transportation network and Ports of Entry (POEs). District 11 works with the federal, state, and local government agencies and community stakeholders from the United States and Mexico, in addition to representing Caltrans in a number of national and binational forums.

MEXICO: CALIFORNIA'S TOP TRADING PARTNER

Trade is a key contributor to local, state, and national economic growth, and land POEs play a critical role in maintaining this growth. In 1999, Mexico surpassed Japan to become California's top trading partner. In 2010, over one million trucks transported goods valued at over \$53 billion through California's POEs. The highway system carries 98 percent of freight flows, and the system is strained at key bottlenecks (e.g., POEs).

The forecast for San Diego County border crossings in 2050 is over 3.4 million incoming trucks and 39 million tons of goods, valued at \$309 billion (an average annual growth of 5.3 percent in value, between 2007 and 2050). For Imperial County's border crossings, almost 17 million tons of goods are forecasted, valued at \$143 billion (an average annual growth of 504 percent).

TRADE CORRIDORS IMPROVEMENT FUND (TCIF) SUPPORTED PROJECTS

This is a state fund that supports projects that improve infrastructure along federally designated "Trade Corridors of National Significance" that have a high volume of freight movement. The following are TCIF projects:

- [SR-905](#) project will provide a six-lane freeway to serve the POE and businesses in the Otay Mesa Area.
- [SR-11/Otay Mesa East POE](#) will improve the efficient movement of people, goods, and services between the United States and Mexico.
- [Southline Rail Improvements – Yard Expansion and Mainline Improvements](#) provides the essential rail connection for the region's freight operations on the San Diego & Arizona Eastern Railway (SD&AE) between the U.S.-Mexico border at San Ysidro, the Port of San Diego, and downtown San Diego.
- [SR-78/111 Brawley Bypass](#) will provide continuity between SR-111 and SR-86, a key facility in the International Border Trade Corridor.

INTERNATIONAL BORDER TRADE CORRIDORS (IBTC)

The IBTC system is comprised of routes of statewide significance to facilitate and increase trade, ensure safe cross-border trucking, and improve the multimodal transportation network leading to the major international border crossings. Highways and rail lines are included, providing for the movement

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of both people and goods. In San Diego County, IBTC routes are I-5, I-8, I-15, SR-163, SR-905, SR-125, SR-94, SR-188, and the future SR-11. In Imperial County the IBTC routes are I-8, SR-7, SR-111, SR-78, SR-86, and SR-98.

LAND PORTS OF ENTRY

San Ysidro POE (I-5)

Between San Diego and Tijuana, the San Ysidro POE is rated as the busiest in the western hemisphere, with an average of 50,000 northbound vehicles and 20,000 northbound pedestrians crossing daily. San Ysidro does not process commercial vehicles.

Otay Mesa POE (SR-905)

Otay Mesa is the third busiest commercial POE on the U.S.-Mexico border (by trade value) and the busiest in California, handling about 1.4 million trucks and over \$20 billion worth of goods in both directions annually. The existing commercial POE is inefficient, with import and export facilities separated by the main passenger northbound and southbound lanes. Export trucks form lengthy queues on city streets within San Diego.

Proposed Otay Mesa East POE and SR-11

Otay Mesa East is a new \$722.4 million POE, scheduled to open in 2015, that will add border crossing capacity, including 2.1 miles of a new, four-lane tolled highway (SR-11). Also part of the project is a Commercial Vehicle Enforcement Facility. Otay Mesa East will help reduce traffic congestion at the San Ysidro, Otay Mesa, and Tecate POEs by providing a new border crossing alternative.

Tecate POE (SR-188 and SR-94)

Located in rural San Diego County about 40 miles east of downtown San Diego, Tecate POE provides service for pedestrians, passenger vehicles, and commercial vehicles. A rail line crosses at Campo, east of the POE). The POE is served by SR-188, a two-lane facility extending 1.9 miles between the border and SR-94. Segments of SR-94 are not built for large trucks and lack passing lanes.

Calexico West POE (SR-111)

The most important non-commercial POE in Imperial County, Calexico West has significant auto and pedestrian activity, resulting in substantial congestion at this POE and along the State highways that access the international border. The U.S.

Customs and Border Protection plans to reconfigure this POE within downtown Calexico.

Calexico East POE (SR-7)

Calexico East serves nearly all of the international truck traffic crossings in Imperial County. U.S. General Service Administration and Caltrans are working together to identify low cost, high impact, expedited implementation of vehicular (passenger and commercial) capacity enhancing projects.

Andrade POE (SR-186)

Located near I-8 and Yuma, Arizona, Andrade is a rural POE with minimal freight. This POE is used primarily by pedestrians from the U.S. wishing to shop at pharmacies or use medical services in Algodones, Baja California.

BORDER ISSUES

- Port of Entry (POE) facilities and routes are severely congested with predicted increases in truck traffic.
- Over 30,000 jobs were lost nationwide due to delays at the California/Mexico border in 2008, including 25,000 in California alone. The impacts of delays were also significant on the other side of the border, with over 11,000 jobs lost in Mexico, including 7,600 in Baja California.
- By 2017, growth projections suggest that nearly 45,000 jobs could be lost in the United States and 16,000 in Mexico due to crossing delays.
- Because of delays at the border, San Diego and Imperial counties lost 6 million and 1.2 million personal trips, respectively, in 2008, resulting in a business revenue loss of \$1.16 billion.
- Border delays were also responsible for productivity losses in the form of lost work hours and lost labor income (\$29.8 million in San Diego County and \$3.4 million in Imperial County) for cross-border commuters from Mexico working in the U.S.
- Currently I-5 and SR-905 experience excessive southbound queues into Mexico. Caltrans estimates peak hour queuing ranging from 45 to 75 minutes into Mexico at the San Ysidro and Otay Mesa POEs. Southbound congestion at I-5 and SR-905 negatively impact the state's binational transportation network's efficiency and reliability.

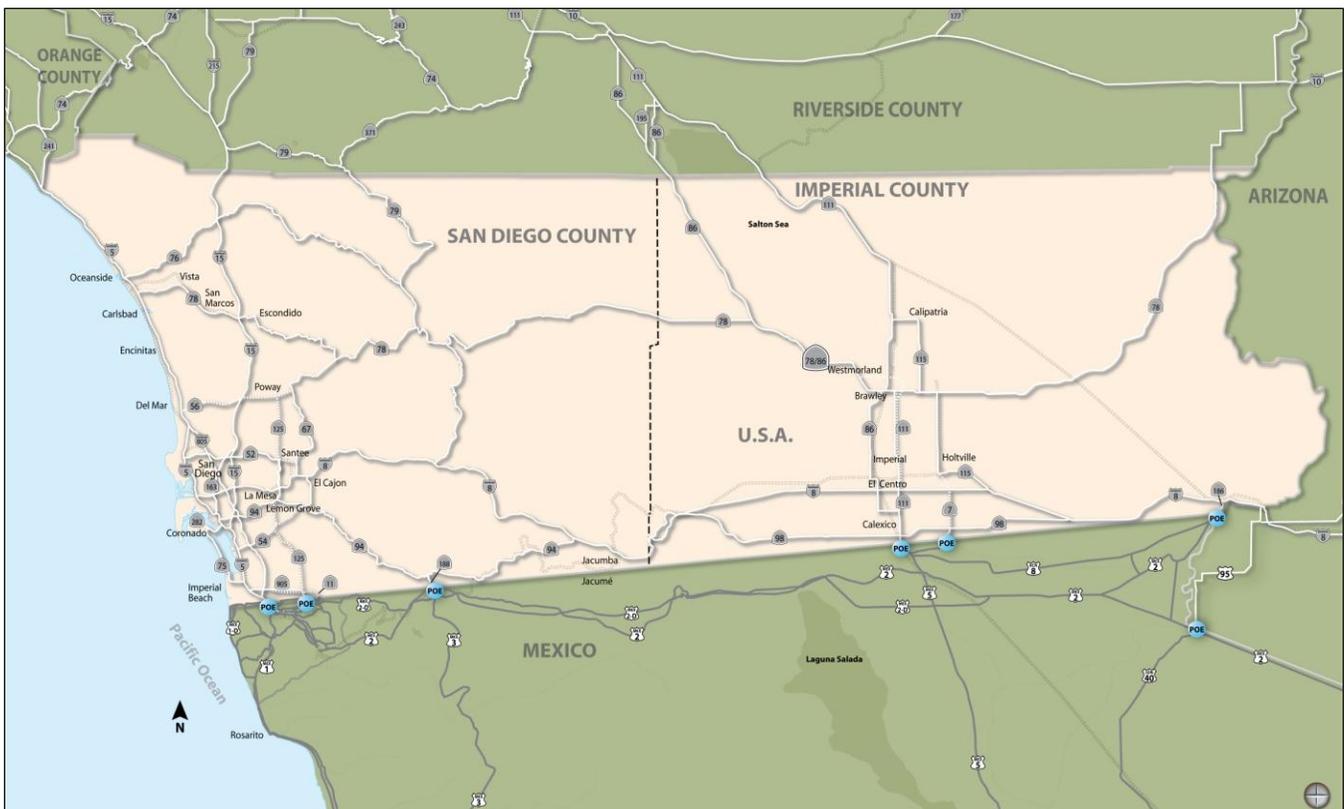
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- U.S. and Mexico recently signed agreements to facilitate long-haul, cross-border trucking between the two countries. Under the agreement, the U.S. will reinstate a pilot program for Mexican truck certification that was introduced under the Bush administration and defunded by Congress in 2009. Mexico, in turn, will immediately drop half of the tariffs on about 100 US products, with the rest to be removed when Mexican trucks actually start crossing the border. Some oppose any trucking accord allowing Mexican trucks to come north due to safety concerns.

BORDER MASTER PLAN (BMP)

The California-Baja California Border Master Plan (BMP) is a bi-national, comprehensive approach to coordinate strategic planning and project delivery at international POEs and connecting transportation infrastructure in the border region. The BMP identifies border-related funding needs for roadways and interchanges in the amount of \$5 billion dollars (2008 dollars) for the 2030 horizon. Additionally, the BMP sets standardized criteria for project evaluation and prioritization and conducts strategic forecasting and analysis of regional transportation needs and trends.

BORDER COUNTIES OF SAN DIEGO AND IMPERIAL



SOURCES AND ADDITIONAL INFORMATION

California Air Resources Board and Business, Transportation & Housing Agency (Goods Movement Action Plan)

<http://www.arb.ca.gov/gmp/docs/gmap-1-11-07.pdf>

North American Transborder Freight Data <http://www.bts.gov/programs/international/transborder/search.html>

California Department of Transportation (Caltrans), Office of Truck Services <http://www.dot.ca.gov/hq/traffops/trucks/>

HDR Decisions Economics, Economic Impacts of Wait Times at the California–Mexico Border 2009 Update

<http://www.dot.ca.gov/dist11/departments/planning/pages/planningproducts.htm>

Southern California Association of Governments (SCAG), <http://www.scag.ca.gov/goodsmove/>

Border Master Plan <http://www.dot.ca.gov/dist11/departments/planning/pages/planningproducts.htm>

San Diego and Imperial Valley Gateway Study

<http://www.dot.ca.gov/dist11/departments/planning/pages/planningproducts.htm>

SANDAG (including Otay Mesa East) <http://www.sandag.org/index.asp?projectid=56&fuseaction=projects.detail>

Trade Corridors Improvement Fund: <http://www.catc.ca.gov/programs/tcif.htm>