

JABRE CONTRACTING Inc.

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License # 981190A

Department of Transportation
Office Engineer MS 43
1727 30th Street,
Sacramento, CA 95816-8041
Fax # (916) 227-6282
Attn: John McMillan, Duty Division Chief

January 13, 2015

Re: 08-0P8204 Rebuttal to Western Structures Bid Protest #3

Dear Mr. McMillan,

We would like to respond to Western Structures rebuttal regarding the use of Mallard and their false accusations regarding our closures.

As mentioned previously Jabre will be setting up both complete closures and shoulder closures when needed for the various work being performed utilizing rented equipment from BRK. It takes 3 workers one in the attenuator the other two in the traffic control truck setting up the signs and placing the cones. These same employees will be removing the trees and bushes for the clearing and grubbing. Our drivers will need assistants in leaving the work area and being told when to come out onto the ramp thru the shoulder closure. These flaggers will not be conducting a reverse traffic control as WS keeps trying to inject. There are others usages of flagging other than a reverse traffic control system. I would like to refer to WS the latest version of the Flagging Instruction Handbook published in October 2014.

Page 1) Flaggers must be trained and prequalified. Mallard yes, Jabre no.

Page 1) Control Signaling Devices (such as paddles and Flags) to provide clear and positive guidance to drivers (our trucks) approaching a temporary traffic control zone (our shoulder closure onto the ramp) in frequently changing conditions.

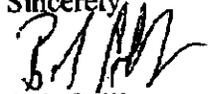
Pages 7-9) Hand Signaling Devices such as Stop/Slow paddles, and Flags. The objective is to tell the driver (our trucks) what actions to take. To be understood by the driver. To take the guess work out of the communication by using standard hand and paddle signals (to our drivers).

Page 11-13) To stop traffic, alert, and proceed traffic (our trucks coming back out onto the ramp).

Jabre Contracting needs two other labors to assist our workers to get the trucks in and out and could have gone to the labor hall for two labors but it is our policy to reach out and assist various DVBE and DBE contractors to participate on our jobs, giving them work and allowing us to reach the states and feds requirements.

It is for these reasons that Jabre Contracting, who is listed as the low bidder on this project be awarded the job and that protest arguments of Western Structures be dismissed.

Sincerely,



Bob Collins

President

Jabre Contracting Inc.